



Detroit Speed
Front Tubular Sway Bar
 1970-1981 Camaro/Firebird and 1964-1977 A-Body
 P/N: 031402DS & 031406DS

The Detroit Speed Front Tubular Sway Bar is designed to address the shortcomings of the small factory Camaro/Firebird & A-Body sway bar. The DSE sway bar kit comes with greaseable polyurethane bushings, mounts, and end links. Testing revealed a dramatic improvement on cars with stock suspension and optimal improvement on cars fitted with other DSE components.



Figure 1 - Part Number 031402DS Shown

Item	Description	Quantity
1	Tubular Sway Bar	1
2	LH Frame Adapter Plate	1
3	RH Frame Adapter Plate	1
4	3/8" - 16 x 3/4" L Countersunk Socket Head Cap Screw	4
5	Polyurethane Sway Bar Bushing	2
6	Sway Bar Bushing Mounting Bracket	2
7	Sway Bar End Links	2
8	3/8" - 16 Nylock Nut	4
9	3/8" AN Washer	4
10	Super Grease	1
11	Instructions	1

Fastener Torque Specification	
Application	Torque (ft-lbs)
Frame Adapter Plate	35
Sway Bar Bushing Retainer (3/8" - 16)	35

1. To begin installation, chock the rear wheels and loosen the front lug nuts. Raise the front of the vehicle and support the front of the vehicle under the control arms so the front suspension is at ride height. Remove the front wheels.

2. Remove the existing sway bar from vehicle. **NOTE:** For the 1964-67 A-Body applications, the factory 5/16"-18 mounting holes will have to be drilled and tapped for the 3/8"-16 hardware.
3. Place the provided frame adapter plates on the frame. The countersunk slotted holes are used to mount the adapter plates to the frame. Thread the 3/8"-16 x 3/4" L Countersunk Socket Head Bolts into the frame using Medium Strength Blue Loctite 242 and torque the bolts to 35 ft-lbs. **NOTE:** The plates need to be positioned so that the studs are closest to the front bumper. The countersunk holes in the adapter plates are slotted, so your bolts will not be centered in the slots. Refer to Figure 2 for reference.

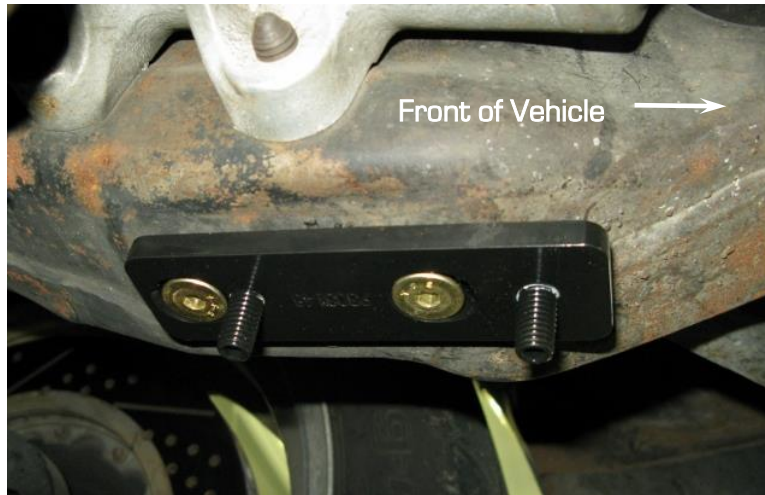


Figure 2 - Mounting the Adapter Plate (1964-67 A-Body)

4. Install the urethane bushings on the sway bar using the provided Super Grease and slide the brackets over the bushings.
5. Position the sway bar against the adapter plates so the brackets go over the studs. Thread the 3/8"-16 Nylock Nuts onto the stud along with an AN washer. Do not tighten at this time. See Figure 3.

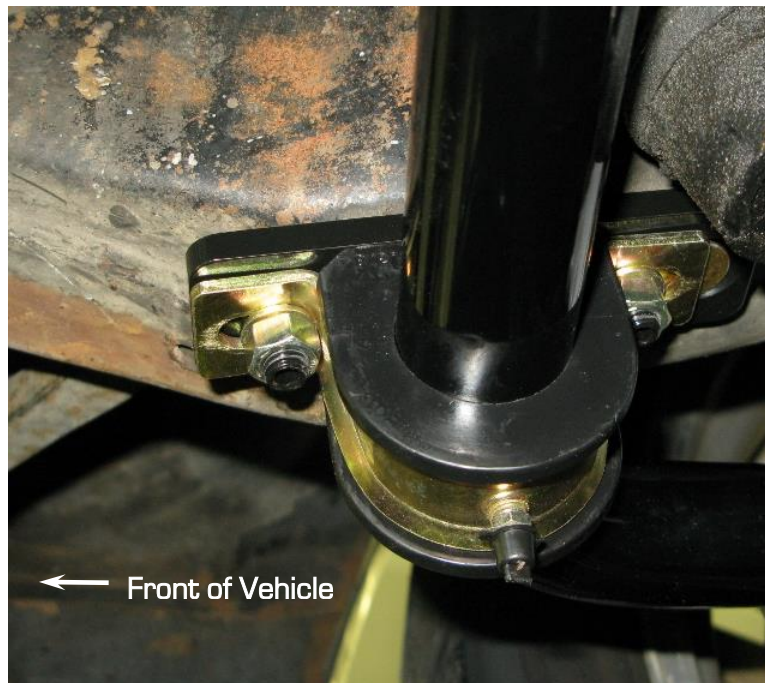


Figure 3 - Install Sway Bar (1964-67 A-Body)

6. Disassemble the sway bar end links and re-assemble the assembly between the sway bar and the lower control arm. **NOTE:** It may be necessary to reposition the sway bar in the mounts to better line up the end links. Do not overtighten the end links. The end links are tight when the polyurethane bushings start to compress.
7. The sway bar bushing clamps can now be tightened to the adapter plates. Torque the nuts to 35 ft-lbs.
8. Reinstall the front wheels and torque to the manufacturer's recommended torque specs. Lower the vehicle to the ground. The finished installation should resemble Figure 4.

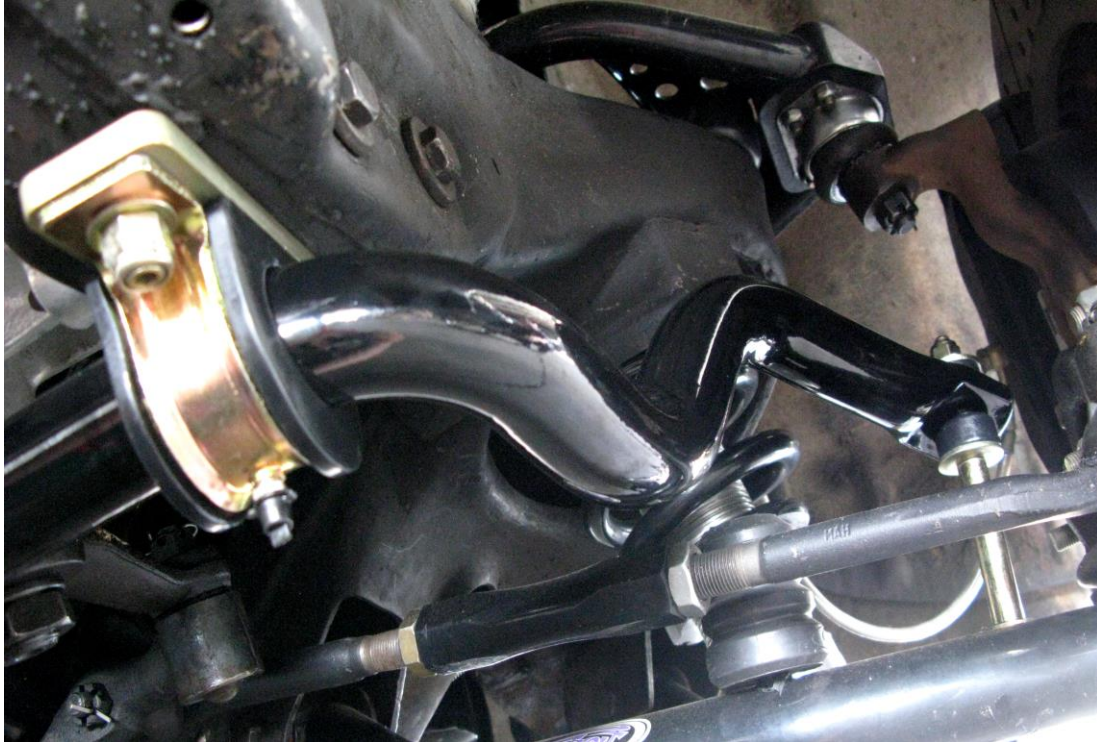


Figure 4 – Completed Installation

If you have any questions before or during the installation of this product, please contact Detroit Speed at tech@detroitsspeed.com or 704.662.3272

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