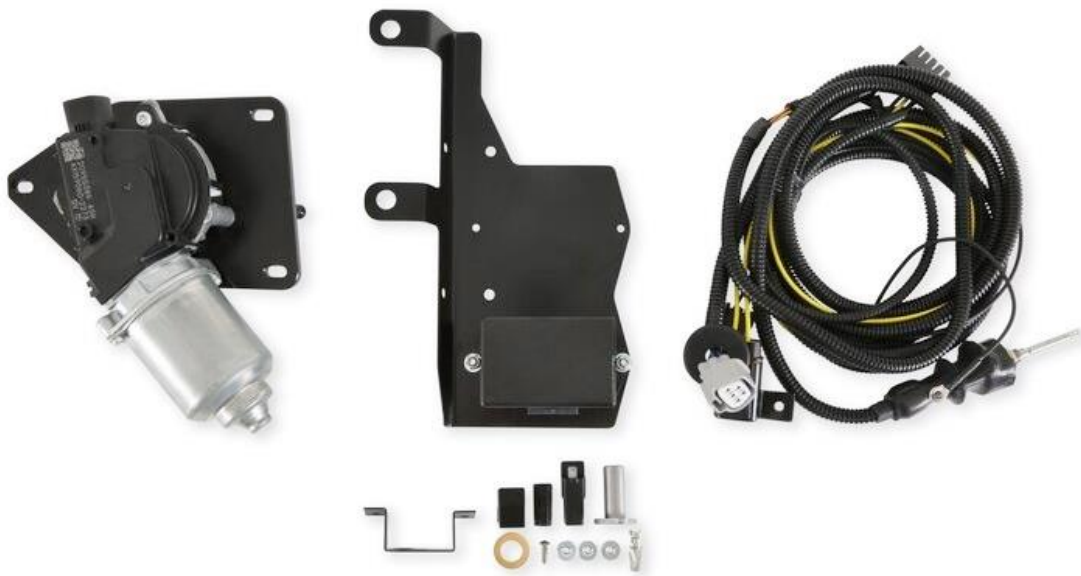


Detroit Speed
Selecta-Speed Wiper Kit
 1973-76 Corvette, 1968-72 Corvette (one-piece hood)
 P/N: 121622

A downpour of rain will no longer hinder your ability to clearly see the road. The Detroit Speed Selecta-Speed Wiper Kit provides you with the performance and convenience of a late model wiper system in a package that easily and cleanly mounts in your 1973-76 C3 Corvette. This DSE wiper kit can be also used on a 1968-72 Corvette with a one-piece hood.



Item #	Description	Quantity
1	Wiper Motor Assembly w/ Adapter Plate	1
2	Wiper Control Module w/ Mounting Plate	1
3	Wiper Harness	1
4	Wiper Switch Assembly	1
5	Billet Aluminum Wiper Switch Knob	1
6	Wiper Switch Adapter	1
7	Drill Guide	1
8	6-32 x 1/2" L Button Head Cap Screw	1
9	1/2" AN Flat Washer	1
10	Packard 56 Series Connector	1
11	10-24 Nylock Flange Nut	3
12	Instructions	1

This kit features a seven-speed wiper system with five delays, low speed, and high speed. A CNC aluminum adapter plate mounts the new wiper motor to the stock firewall bolt pattern. The new pitman arm, included in the kit, connects directly to your existing wiper linkage. A rotary switch is also included, along with a complete wiring harness.

This kit does feature a washer pump option as the wiper switch does have a push button function. The wiring harness/control module is equipped with a power and ground wire to install an inline electric washer pump. DSE does offer a washer pump kit available as part number 121102.

Installation Instructions:

1. Before beginning, please ensure that the parts included with your kit match the parts list above. Ensure that the factory wiper system is in its "Parked" position. Disconnect the battery power by removing the negative battery lead from the battery. Mark the position of the wiper arms by placing masking tape on the windshield. This will assure the arms are accurately replaced.
2. Disconnect both the left and the right side wiper linkage from the wiper motor pitman arm (Figure 1).

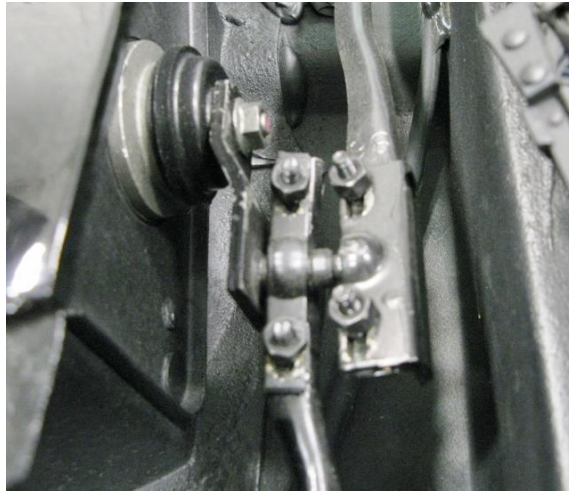


Figure 1 – Remove Wiper Linkage

3. Remove the distributor cap from the housing (leave the ignition wires connected to the cap) if needed and position it out of the way.
4. Disconnect the original wiper motor wiring from the motor along with the windshield washer hoses. The original wiring will not be used with the new Selecta-Speed Wiper Kit. A custom harness is provided to replace the original. **NOTE: If you decide to cut the old wires please pay special attention to properly terminate the wire ends to avoid possible shorting.**
5. Remove the three nuts holding the original wiper motor to the firewall stud plate and remove the wiper motor (Figure 2).



Figure 2 – Remove Original Wiper Motor

6. Mount the new wiper motor assembly to the firewall stud plate using the three supplied 10-24 Nylock flange nuts. **NOTE:** The pitman arm is designed to have a bend (Figure 3).

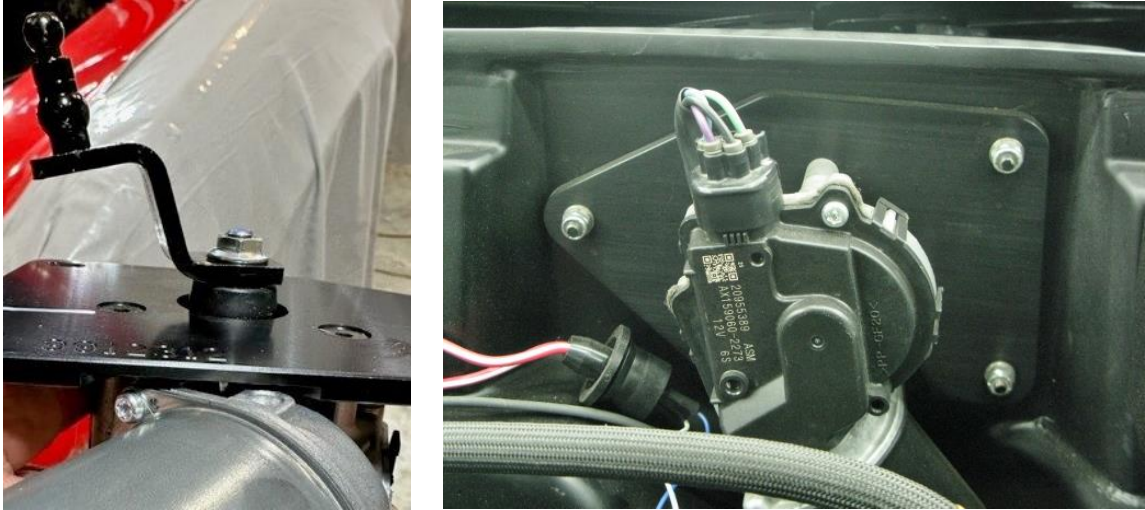


Figure 3 – Attach Wiper Motor to Firewall

7. Attach the new pitman arm to the original wiper linkage. **NOTE: The Selecta-Speed kit is shipped with the pitman arm in the “parked” position.** Do not move the pitman arm by hand to attach the wiper linkage. If the pitman arm is moved from the original “parked” position from DSE, it may result in the wiper blades stopping in the wrong spot on the windshield.



Figure 4 – Attach Pitman Arm

8. The Selecta-Speed switch will replace the stock switch. Remove the four screws holding the center dash bezel in place. Remove the factory wiper knob from the switch and unplug the wiper switch. Remove the two screws holding the switch in the center dash bezel and remove the stock wiper switch.
9. Place the provided 6-32 hex nut into the provided drill guide (Figure 5 on the next page).

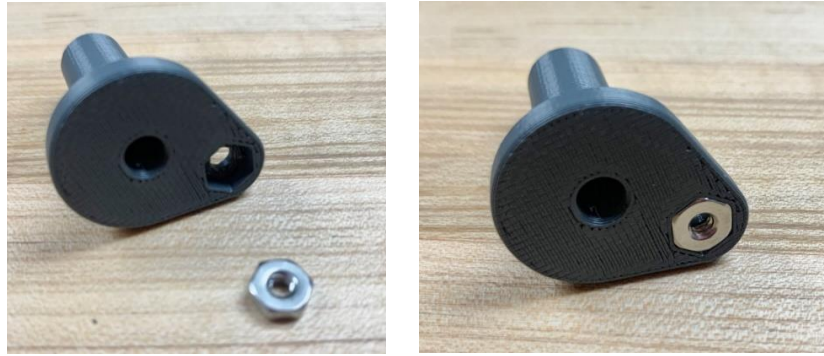


Figure 5 - Install Hex Nut in Drill Guide

10. Place the drill guide into the wiper switch adapter and hold in place with the provided 6-32 x 1/2" L button head cap screw (Figure 6).



Figure 6 - Install Drill Guide into Switch Adapter

11. With the center dash bezel out of the vehicle, install the wiper switch adapter using the two screws from **Step 8** that held the stock wiper switch in place. **NOTE:** Make sure the anti-rotation hole is above the switch hole in the adapter.
12. Using the drill guide, drill a hole into the center dash bezel up to 7/32" for the DSE wiper switch (Figure 7). Remove the drill guide.

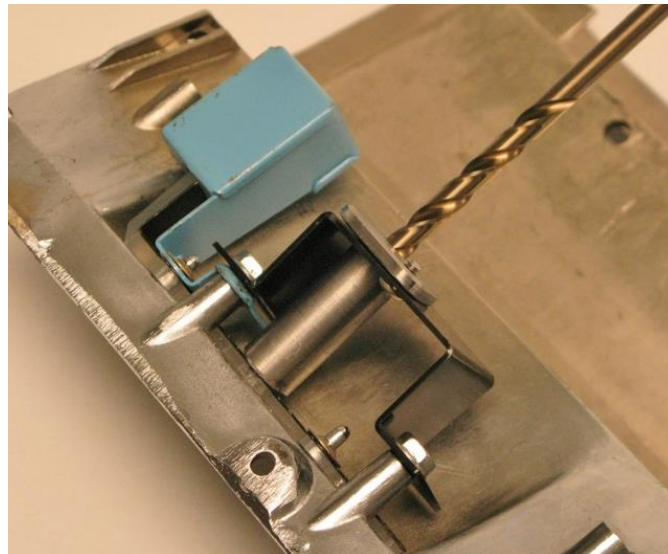


Figure 7 - Drill Center Dash Bezel

13. Remove the hex nut and star washer from the DSE wiper switch and install the provided 1/2" AN washer over the switch.
14. Install the DSE wiper switch into the switch adapter and slide the hex nut on over the switch shaft. Thread the hex nut into the threads of the wiper switch.
15. With the anti-rotation tab of the switch locked in place on the switch adapter, tighten the hex nut onto the wiper switch.
16. Slide the DSE wiper knob onto the wiper switch shaft. Align the set screw with the flat on the wiper switch and tighten (Figure 8). **NOTE:** Make sure the set screw locks onto the flat on the wiper switch.

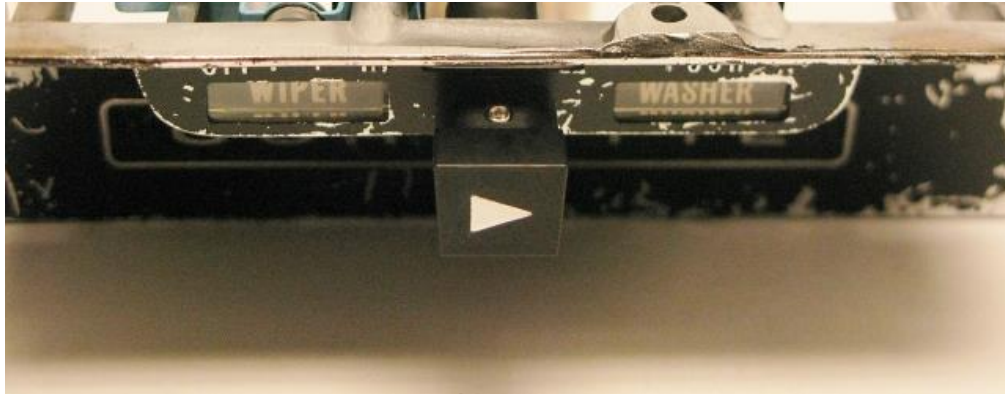


Figure 8 – Install DSE Wiper Knob

17. Re-install the center dash bezel back into the dash. Route the wiper switch harness to the left hand side of the vehicle. **NOTE:** For the 1968-69 vehicles that were equipped with an under dash cross brace, you will need to notch the back edge of the brace to allow for clearance at the back of the wiper switch. For factory A/C vehicles, you may also need to clearance the center vent duct (Figure 9).

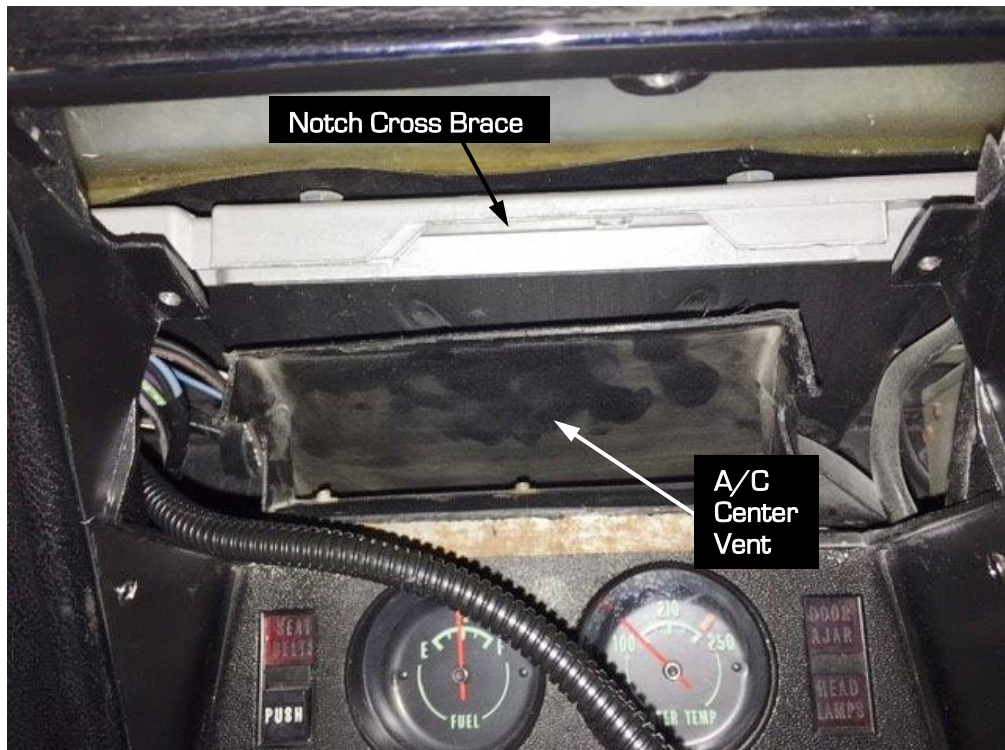


Figure 9 – 1968-69 C3 Cross Brace

18. The gauge cluster will need to be dropped down in order to install the wiper control module. **NOTE:** DSE recommends first removing the steering column. This is recommended to prevent damaging the gauge cluster while installing the control module assembly. It is possible to remove the gauge cluster without taking out the column however it will make the job much more difficult. If you drop the steering column, the gauge cluster will sit on the column and distort the area around the lower collar. This could cause the gauge cluster to break or crack the section of the gauge cluster between the speedometer and tachometer.
19. To remove the steering column, start by removing the two screws from the lower column cover. **NOTE:** There may be three screws if you have an aftermarket steering column. You may find the vacuum override switches and/or the wiper override electrical switch (1968-72) attached to the cover (Figure 10). Remove the lower left hand side dash vent duct.

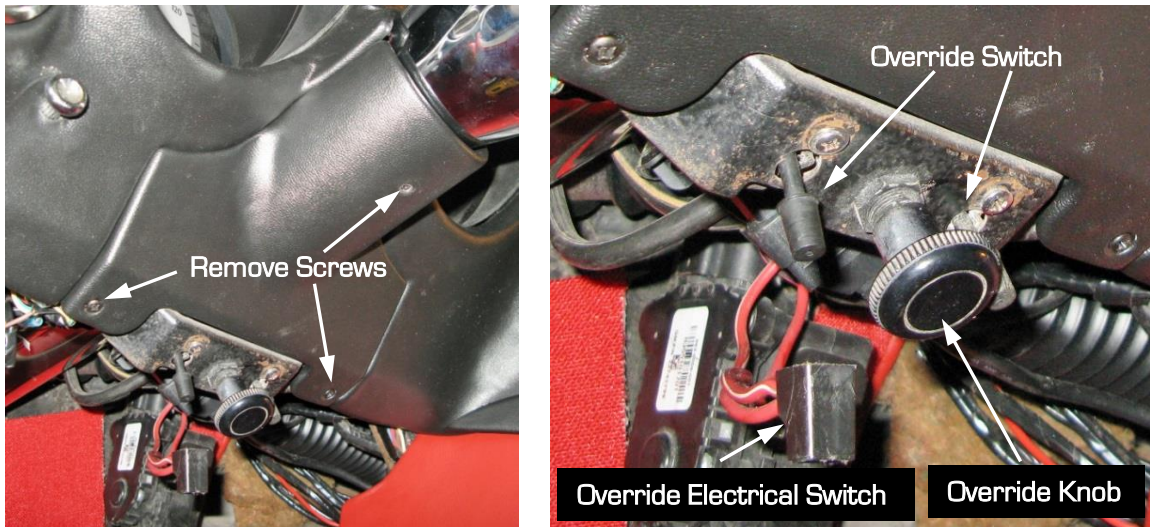


Figure 10 - Remove Lower Column Cover

20. Remove the two bolts holding the steering column to the support brace, Remove the two carriage bolts from the lower part of the column (Figure 11). **NOTE:** The steering column used in the pictures below is from an aftermarket column.

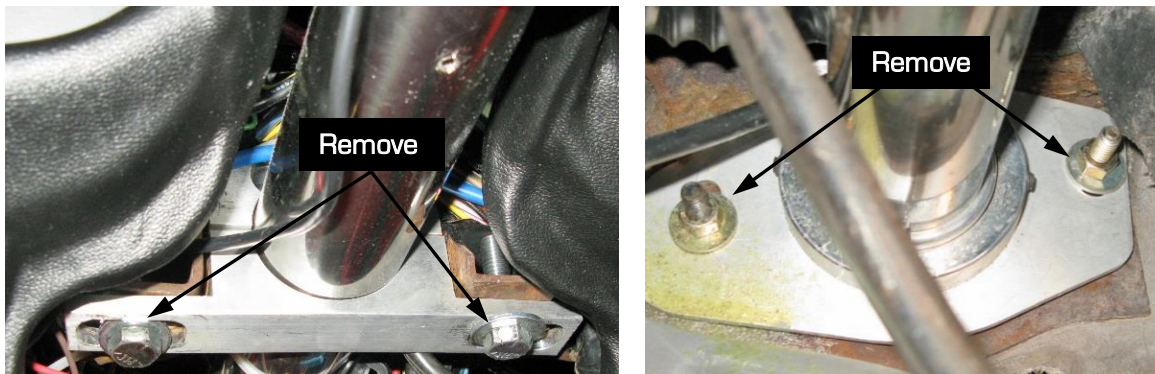


Figure 11 - Remove Steering Column Hardware

21. Remove the clips from the interlock cable pin and cable, and remove the cable from the lower column. Remove the bolt holding the "tulip flange" coupler to the lower column and separate the two by carefully prying them apart with a screwdriver. If you have a splined U-joint, loosen the jam nut and set screw before removing the column.

22. Remove the electrical (ignition switch) switch connection from the lower part of the steering column. There are two halves to this connector however they will come out as one. Pull the steering column out of the car. You may need to continue to pry between the coupler and the column and wiggle the column loose. **NOTE:** With an aftermarket column you can pull the steering column out of the coupler while resting the steering wheel on the front seat (Figure 12). By not completely removing the column from the vehicle, you can leave the electrical switch connected to the column.

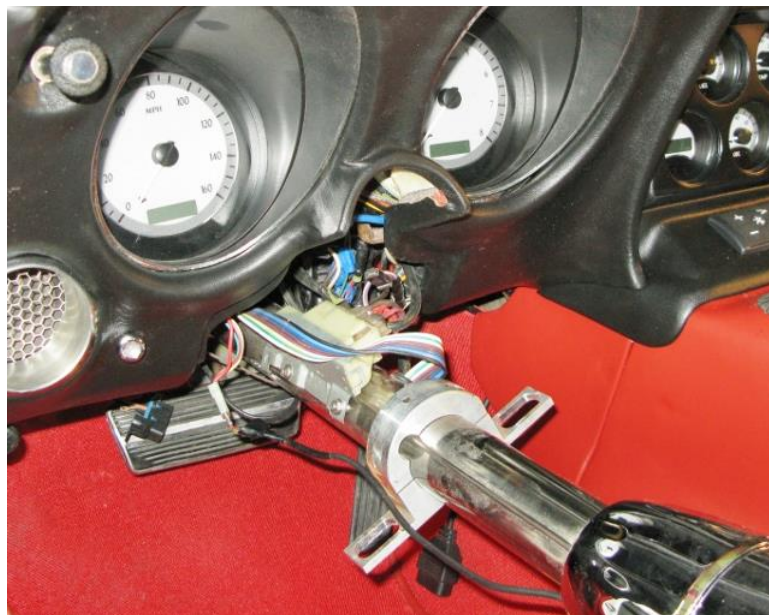


Figure 12 – Remove Steering Column

23. With the steering column out of the way, remove the gauge cluster. Remove the three screws around the top of the pad and two screws on each side of the gauge cluster. Once all the screws are removed, it should be ready to drop down (Figure 13). **NOTE:** For the 1968-74 application, you should remove the tachometer cable from the distributor. For all applications you should remove the speedometer cable from the transmission. You will be able to pull these cables out with the gauge cluster instead of having to reach behind the gauge cluster and unscrew or unclip them from the gauges.



Figure 13 – Remove Gauge Cluster

24. Install the wiper control module onto the module mounting plate using the provided 8-32 hardware if not already assembled from DSE. (Figure 14). Do not overtighten. **NOTE:** If you also have the DSE headlight kit, you can mount both control modules to the same mounting plate (Figure 15). Depending on your application, your module mounting plate may look slightly different than the pictures below.

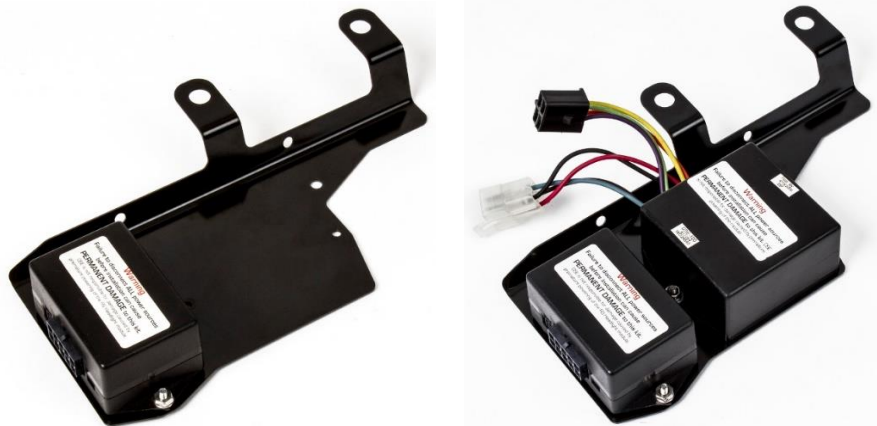


Figure 14 - 73-82 C3 Wiper Module Figure 15 - 73-82 C3 Wiper & Headlight Module

25. Mount the wiper control module and the mounting plate to the steering column support. Remove the two bolts from the column support and mount the module and mounting plate to the support using the two bolts that were removed (Figure 16). There are extra holes in the mounting plate so you can wire tie your harness to the mounting plate. **NOTE:** For the 1968-69 vehicles that were equipped with an under dash cross brace, you may need to bend the module plate down slightly away from the brace.



Figure 16 - Mount Wiper Module (Headlight Module also Shown)

26. Route the wiper motor connector side of the wiring harness (Figure 17) through the firewall using an existing hole if possible. Otherwise you will need to drill a 1-1/4" hole to allow the wiper motor connector to pass through the firewall.



Figure 17 - Wiper Motor Connector

27. A rubber grommet is already installed on the wire loom for the wiper motor wires. Once there is enough loom through the firewall to connect to the wiper motor, move the grommet on the loom and install it into the firewall to seal the engine compartment from the inside of the vehicle.

28. Install the wiper motor connector to the wiper motor (Figure 18).



Figure 18 - Install Wiper Motor Connector

29. The DSE Selecta-Speed harness does include a weather pack connector to install an optional electric inline washer pump into your vehicle (Figure 19). DSE does offer a washer pump kit you can purchase separately as part number 121102.



Figure 19- Washer Pump Connection

30. If you have purchased the Detroit Speed washer pump kit, use the instructions from that kit to complete the washer pump kit installation. If you do not want to use the washer pump feature, you can tie wrap this connector/loom up under the dash.
31. With the wiper control module in place, plug in the wiper switch and wiring harness connectors into the control module (Figure 20).

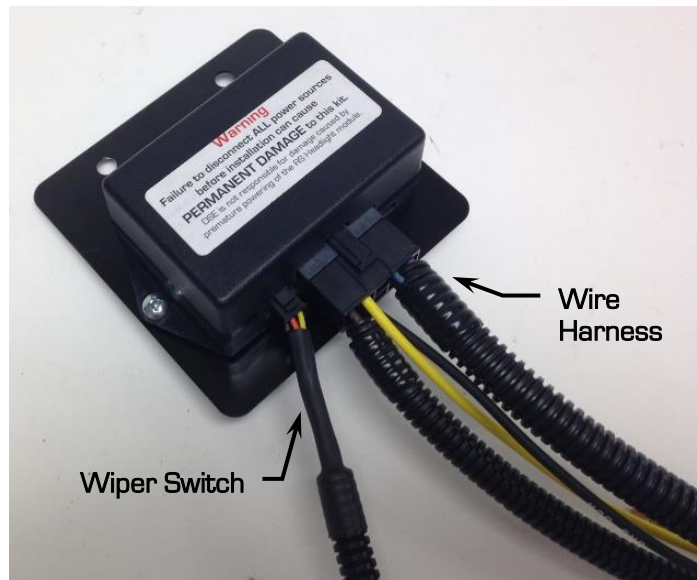


Figure 20– Connect Wiper Switch & Harness to Module

32. Re-install the gauge cluster and the steering column by reversing the process described in **Steps 19 through 23**.
33. Connect the yellow wire under the dash to a 12V ignition or accessory switched source. (Circuit is active when the key is in the run position). This lead is supplied with an inline 30 amp ATO fuse.
34. Connect the black wire with the round eyelet to a ground located under the dash. Make sure a proper ground is obtained by removing any rust or paint from the metal.
35. Connect the negative battery lead and test the wiper system. Upon a successful test, the wiper conversion is now complete. Secure the new wiring harness under the dash and in the engine compartment. Re-install any other components that have been removed.

NOTE: In some cases high energy ignition systems have caused interference with the correct operation of the Selecta-Speed Wiper Kit due to spark plug wires routed closely to the wiper motor. If this occurs, re-routing your spark plug wires may be necessary.

If you have any questions before or during the installation of this product, please contact Detroit Speed at tech@detroitsspeed.com or 704.662.3272

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